

|            |         |          |       |             |              |                  |
|------------|---------|----------|-------|-------------|--------------|------------------|
| DESIGN NO. | DATE    | DESIGNER | SCALE | PROJECT NO. | PROJECT NAME | PROJECT LOCATION |
| 7          | 1/15/35 |          |       |             |              |                  |

# KENTUCKY DEPARTMENT OF HIGHWAYS

## BRIDGE OVER LOST CREEK

### UNION COUNTY

#### LIST OF DRAWINGS

1. TITLE SHEET & ESTIMATE OF QUANTITIES
  2. LAYOUT
  - 3 & 4. END PNTS 1 & 2 & ANCHOR BOLT PLAN
  5. PIER 1
  6. PIER 2
  7. SPANS 1, 2, & 3
  8. "
  9. "
  10. "
  11. SHOES
  12. EXPANSION DAMS & DRAINS
  13. HANDRAIL
  14. ELEVATIONS
  15. SOUNDINGS
- P-17  
G-351

| ITEM            | CONCRETE (CU YD) |       | REINFORCE-  | STRUCTURAL | STR. EXC. (CU YD) |       | 12" B.P. @ 53 LBS. |         | SLOPE PROTECTION | REMOVAL OF   |
|-----------------|------------------|-------|-------------|------------|-------------------|-------|--------------------|---------|------------------|--------------|
|                 | CLASS            | CLASS | MENT (LBS.) |            | COMMON            | S. R. | FURNISHING         | DRIVING |                  |              |
| END BENT No. 1  | 44.5             | 1.2   | 4176        |            | 10                |       | 408                | 408     |                  | Lump Sum Bid |
| PIER No. 1      | 35.6             |       | 15,893      |            | 500               | 30    |                    |         |                  |              |
| PIER No. 2      | 93.1             |       | 16,500      |            | 500               | 35    |                    |         |                  |              |
| END BENT No. 2  | 44.5             | 1.4   | 4176        |            | 5                 |       | 370                | 370     | 130              |              |
| SPANS 1, 2, & 3 | 254.2            |       | 57708       | 8760       |                   |       |                    |         |                  |              |
| HANDRAIL        |                  | 13.7  | 2,612       |            |                   |       |                    |         |                  |              |
| TOTALS          | 531.7            | 16.5  | 101,065     | 8760       | 1015              | 65    | 778                | 778     | 130              |              |

#### NOTE:

Structural steel weight includes structural shapes, bolts, lead plates, and C.I. roadway drains and includes 1 1/2% overrun on structural steel. Bolts, lead plates, and roadway drains will be paid for at the unit price bid for structural steel.

| BILL OF INCIDENTAL MATERIAL |               |                                           |                |
|-----------------------------|---------------|-------------------------------------------|----------------|
| ITEM                        | NO. OF PIECES | SIZE                                      | LOCATION       |
| 1/2" Rem. Exp. Bolt         | 4             | 1-2" x 2 1/2"                             | Key-End Bent 1 |
| 1/2" " " "                  | 4             | 1-2" x 2 1/2"                             | " " " 2        |
| 1/2" " " "                  | 2             | 1-2" x 2 1/2"                             | " " " 1        |
| 1/2" " " "                  | 2             | 1-2" x 4 1/2"                             | " " " 2        |
| 1" " " "                    | 1             | 3/4" x 22 1/8"                            | B.M. " " 2     |
| 1" " " "                    | 2             | 1-2" x 5 1/2"                             | B.M. " " 2     |
| Copper strip                | 1             | 1 1/2" x 12 1/2" See detail G-351 - Rely. | " " " " 2      |
|                             | 2             | 1 1/2" x 3 1/2" " " " " - Sidewalks       | " " " " 2      |

Note:  
Quantities shown in Bill of Incidental Material are approximate only and the contractor is responsible for furnishing enough material to complete the work in accordance with plans and specifications.

G-351  
Pr.

BRIDGE OVER LOST CREEK SHEET 1 OF 13

**COMMONWEALTH OF KENTUCKY**  
DEPARTMENT OF HIGHWAYS  
FRANKFORT  
COUNTY OF  
**UNION**  
UNIONTOWN - SPRING GROVE

5+73 ROAD PROJECT NO. 3340 (2)

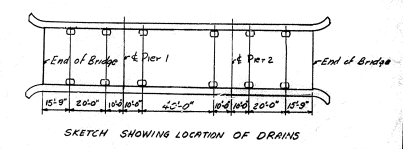
STATION 5+73 BRIDGE NUMBER 18220

DESIGNED BY: DATE: 1/15/35  
 CHECKED BY: DATE: 1/15/35  
 DRAWN BY: DATE: 1/15/35  
 IN CHARGE: DATE: 1/15/35  
 APPROVED BY: DATE: 1/15/35

| REV. | DATE | BY | CHKD. | APP'D. |
|------|------|----|-------|--------|
| 7    |      |    |       |        |

**GENERAL NOTE**

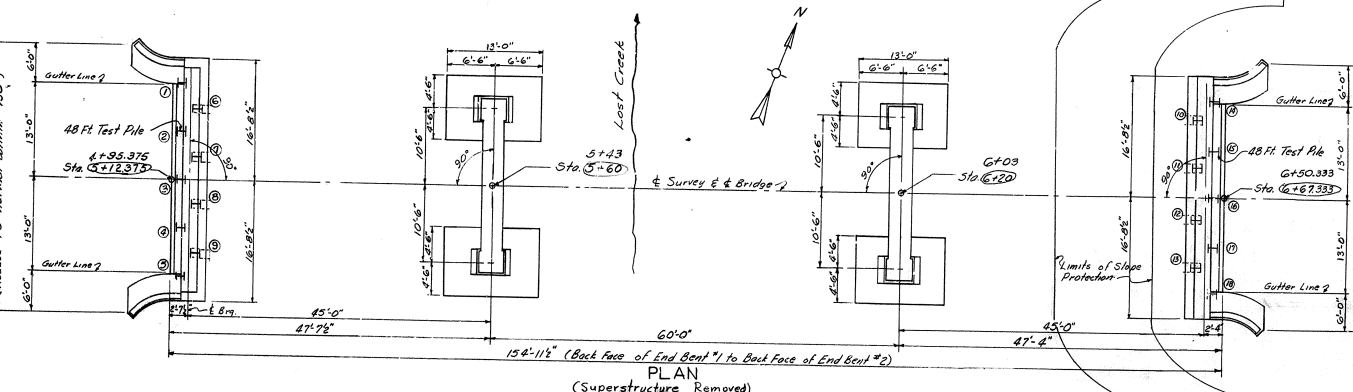
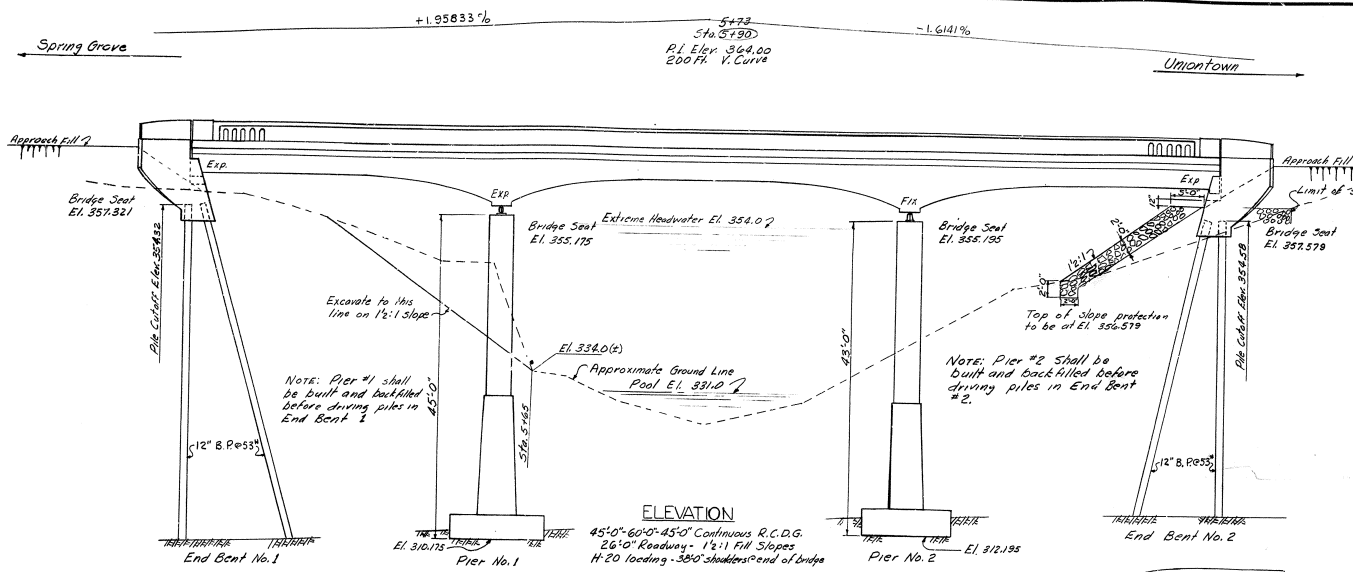
Specifications: Kentucky Department of Highways, 1945 Standard with Amendments  
 Design Load: Bridge designed for No. 44 loading as specified in A.S.T.M. 1933 Specifications.  
 Concrete: Class "C" concrete to be used throughout except in handrail, Class "D" concrete to be used in handrail, Class "F" and Class "G" concrete on the Bridge Superstructure, above the construction joint of beams and underside of floor slab, shall be made with normal Portland Cement, to which has been added the mixer, as approved an ordinary admixture, as specified by Amendment G-6, Article 9.11.  
 Reinforcement: Dimensions from face of concrete to bars are clear distances except as otherwise shown. Dimensions for bar spacing are distances center to center of bars. The deformations of all reinforcing bars shall conform to A.S.T.M. A-305 current specifications.  
 Bared Edges: All exposed edges shall be bared to unless otherwise noted. Expansion Joint Material and Copper Strip: The cost of these items to be included in the unit price for Class "C" concrete.  
 Construction Joints: All construction joints shall be carefully formed and substantial keys provided. The contractor shall furnish sufficient mixer capacity on the job to place the concrete between the construction joints as noted on the plans in a continuous run exceeding ten hours continuously run. After one section of the concrete has been placed the construction joint shall be thoroughly cleaned of all laitance, loose and foreign material, just before the concrete takes its final set (which is about 6 hours). The joint shall then be covered with burlap and kept completely saturated with water. Flush the joint with 1/2 Portland cement mortar before placing the adjoining concrete section.  
 Falsework: The falsework supporting the beams and slab in any span of the continuous unit shall not be removed until all beams and slab of the continuous unit have been poured for at least twenty-one (21) days.  
 Welding: See Sheet No. 12.  
 Slope Protection: To consist of stone dumped to a minimum depth of two feet. Size of stone same as used in cyclopath masonry according to specification 1261.  
 Piling: Piling to be 12" Bearing Piles. Test piles shall be accurately located so as to be used in the finished structure. Piles to be driven to refusal.  
 Remove Existing: Existing 3 span (80'-50'-80') I-Beam bridge, with timber floor and concrete wing abutments for center span is to be removed in accordance with the plans and specifications. The I-Beams shall be removed carefully, without damage, and piled in a location above high water, as directed by the Engineer. The remainder of the structure shall be removed in accordance with Section 6.11.2. of the Specifications. The cost of completing the above work shall be included in a lump sum bid.



Bridge Over Lost Creek Sheet 2

**COMMONWEALTH OF KENTUCKY**  
 DEPARTMENT OF HIGHWAYS  
 FRANKFORT  
 COUNTY OF  
**UNION**  
 UNIONTOWN - SPRING GROVE  
 5+73 ROAD  
 STATION 5+90 PROJECT NO. S-345 (2)

|               |      |
|---------------|------|
| BRIDGE NUMBER | DATE |
| 12820         | 1920 |

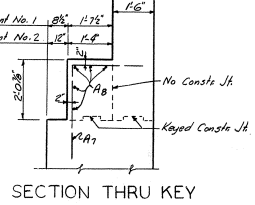
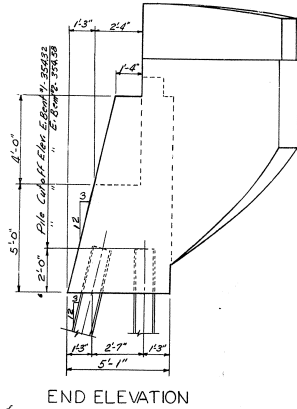
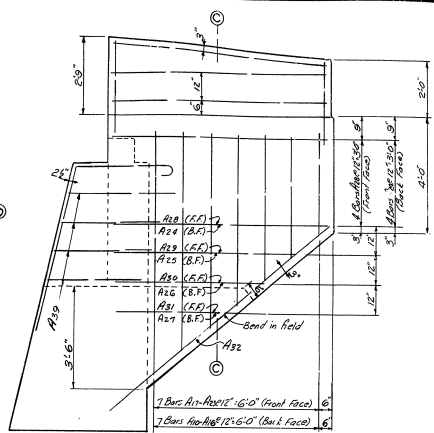
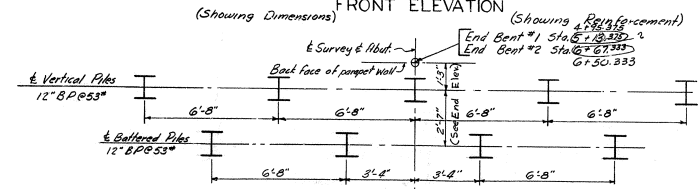
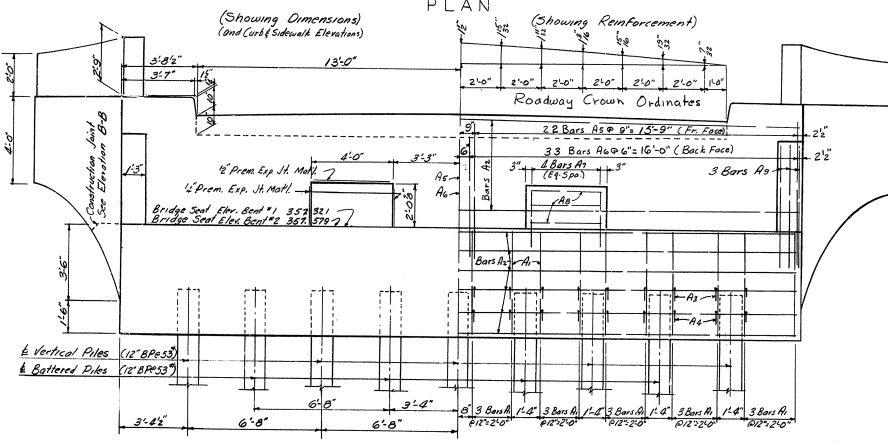
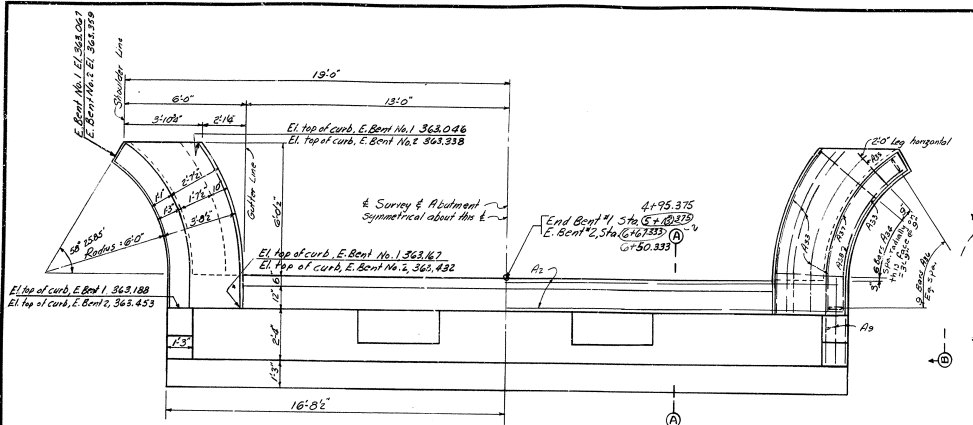


| PILE RECORD |                                |                                 |                                 |                             |          |                                |                                 |                                 |                             |
|-------------|--------------------------------|---------------------------------|---------------------------------|-----------------------------|----------|--------------------------------|---------------------------------|---------------------------------|-----------------------------|
| PILE No.    | CUTOFF ELEV. AS SHOWN ON PLANS | TIP OF PILE ELEVATION AS DRIVEN | PILE LENGTH IN PLACE (LIN. FT.) | CALCULATED BEARING CAPACITY | PILE No. | CUTOFF ELEV. AS SHOWN ON PLANS | TIP OF PILE ELEVATION AS DRIVEN | PILE LENGTH IN PLACE (LIN. FT.) | CALCULATED BEARING CAPACITY |
| 1           | 354.32                         |                                 |                                 |                             | 10       | 334.38                         |                                 |                                 |                             |
| 2           | "                              |                                 |                                 |                             | 11       | "                              |                                 |                                 |                             |
| 3           | "                              |                                 |                                 |                             | 12       | "                              |                                 |                                 |                             |
| 4           | "                              |                                 |                                 |                             | 13       | "                              |                                 |                                 |                             |
| 5           | "                              |                                 |                                 |                             | 14       | "                              |                                 |                                 |                             |
| 6           | "                              |                                 |                                 |                             | 15       | "                              |                                 |                                 |                             |
| 7           | "                              |                                 |                                 |                             | 16       | "                              |                                 |                                 |                             |
| 8           | "                              |                                 |                                 |                             | 17       | "                              |                                 |                                 |                             |
| 9           | "                              |                                 |                                 |                             | 18       | "                              |                                 |                                 |                             |

NOTE: This pile record does not replace other records of piles required to be kept and submitted by the Resident Engineer. After all piles have been driven, the Engineer shall record the tip of pile elevation as driven, length of pile in place, and calculate the bearing capacity of each pile, and return one like print copy of this sheet with this data to the Bridge Engineer to be recorded on the original plans. Length of piles in place shown hereon are the actual length of piles in the finished structure below cutoff elevation and is not necessarily the pay item.

DESIGNED BY: [Name] DATE: [Date]  
 CHECKED BY: [Name] DATE: [Date]  
 REVIEWED BY: [Name] DATE: [Date]  
 APPROVED BY: [Name] DATE: [Date]

DESIGNED BY: J.R.O. CHECKED BY: J.R.O. DATE: 2-1-53  
 DRAWN BY: E.C.W. CHECKED BY: E.C.W. DATE: 2-1-53  
 REVISIONS: 1. Standard Changes, C&B, 1-10-53

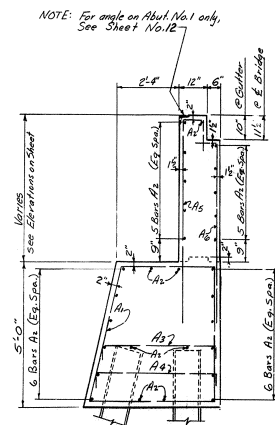
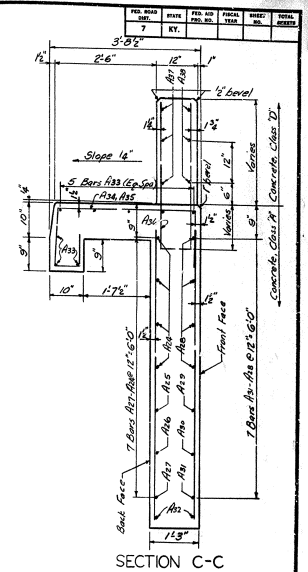


ESTIMATE OF QUANTITIES

Concrete, Class 70  
 Reinforcement  
 12" B.P.E.53, Furnishing  
 12" B.P.E.53, Driving  
 Concrete, Class "O"

End Bent #1  
 44.5 Cu. Yds.  
 4776 Lbs.  
 408 Lin. Ft.  
 808 Lin. Ft.  
 1.4 Cu. Yds.

End Bent #2  
 44.3 Cu. Yds.  
 4776 Lbs.  
 370 Lin. Ft.  
 370 Lin. Ft.  
 1.4 Cu. Yds.



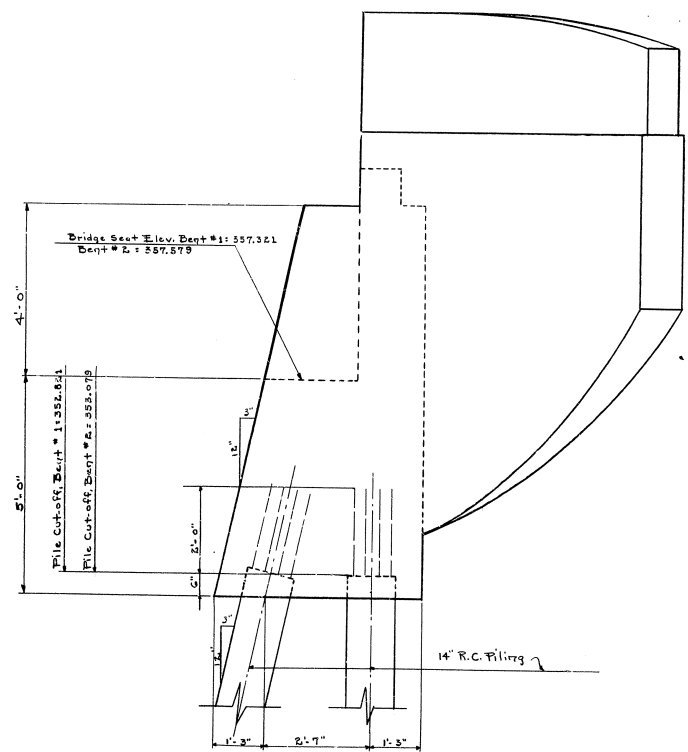
Bridge Over Lost Creek Sheet 3

**COMMONWEALTH OF KENTUCKY**  
 DEPARTMENT OF HIGHWAYS  
 FRANKFORT  
 COUNTY OF  
 UNION  
 UNIONTOWN - SPRING GROVE

5+73 ROAD  
 STATION 5+73  
 BRIDGE NUMBER 5+73D PROJECT NO. S-345 (2)

DRAWING NO. R2520

DRAWN BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 CHECKED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 DESIGNED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 APPROVED BY: \_\_\_\_\_ DATE: \_\_\_\_\_  
 PROJECT: **FILE RECORD PILE DR. D.T.O. DATE 3/25/59**  
 BRIDGE: **BRIDGE OVER I-63, PILES (GENERAL) 1/12/58**  
 SHEET: **12 OF 12**



**END ELEVATION**  
(End bent #1 & 2)

**ESTIMATE OF QUANTITIES**

|            |                                                                                                                                        |            |                |         |                |
|------------|----------------------------------------------------------------------------------------------------------------------------------------|------------|----------------|---------|----------------|
| Bent No. 1 | <table border="0"> <tr> <td>Furnishing</td> <td>415.0 Lin. ft.</td> </tr> <tr> <td>Driving</td> <td>4000 Lin. ft.</td> </tr> </table>  | Furnishing | 415.0 Lin. ft. | Driving | 4000 Lin. ft.  |
|            |                                                                                                                                        | Furnishing | 415.0 Lin. ft. |         |                |
| Driving    | 4000 Lin. ft.                                                                                                                          |            |                |         |                |
| Bent No. 2 | <table border="0"> <tr> <td>Furnishing</td> <td>375.0 Lin. ft.</td> </tr> <tr> <td>Driving</td> <td>361.0 Lin. ft.</td> </tr> </table> | Furnishing | 375.0 Lin. ft. | Driving | 361.0 Lin. ft. |
|            |                                                                                                                                        | Furnishing | 375.0 Lin. ft. |         |                |
| Driving    | 361.0 Lin. ft.                                                                                                                         |            |                |         |                |

| File No.              | Cutoff Elev. as shown on plans | Tip of pile Elevation as driven | Pile length in place (lin. ft.) | Calculated bearing Capacity |
|-----------------------|--------------------------------|---------------------------------|---------------------------------|-----------------------------|
| <b>END BENT No. 1</b> |                                |                                 |                                 |                             |
| 1                     | 352.821                        | 305.99                          | 46.83                           | 15                          |
| 2                     | "                              | 308.37                          | 44.45                           | 109                         |
| 3                     | "                              | 308.23                          | 44.59                           | 78                          |
| 4                     | "                              | 309.02                          | 43.80                           | 67                          |
| 5                     | "                              | 309.26                          | 43.56                           | 75                          |
| 6                     | "                              | 309.22                          | 44.34                           | 54                          |
| 7                     | "                              | 309.27                          | 44.55                           | 50                          |
| 8                     | "                              | 309.64                          | 42.51                           | 67                          |
| 9                     | "                              | 309.55                          | 44.60                           | 67                          |
| <b>END BENT No. 2</b> |                                |                                 |                                 |                             |
| 10                    | 352.019                        | 311.90                          | 40.11                           | 80                          |
| 11                    | "                              | 311.68                          | 40.33                           | 80                          |
| 12                    | "                              | 311.72                          | 40.29                           | 80                          |
| 13                    | "                              | 312.03                          | 40.21                           | 80                          |
| 14                    | "                              | 311.61                          | 40.40                           | 120                         |
| 15                    | "                              | 311.56                          | 40.45                           | 120                         |
| 16                    | "                              | 310.60                          | 41.41                           | 120                         |
| 17                    | "                              | 311.02                          | 41.00                           | 109                         |
| 18                    | "                              | 311.09                          | 40.91                           | 133                         |

**GENERAL NOTE**

Piling to be 14" R.C. piles as per std. drwg. P. 2. Test piles shall be accurately located so as to be used in the finished structure. All piles shall be driven to rock or refusal, or to sustain a min. bearing of 50 tons per pile.  
 The unit price per lin. ft. of R.C. piles in place, furnished and driven, shall be the same as the contractor's original unit price bid per lin. ft. of steel bearing pile, 12" @ 33", furnished and driven.  
 All piling shall have 20 mm. penetration, jacking to be used if necessary to obtain this penetration.  
 The location of all piling shall be the same as that shown in the pile layout, sheet 3, drwg. # 12550

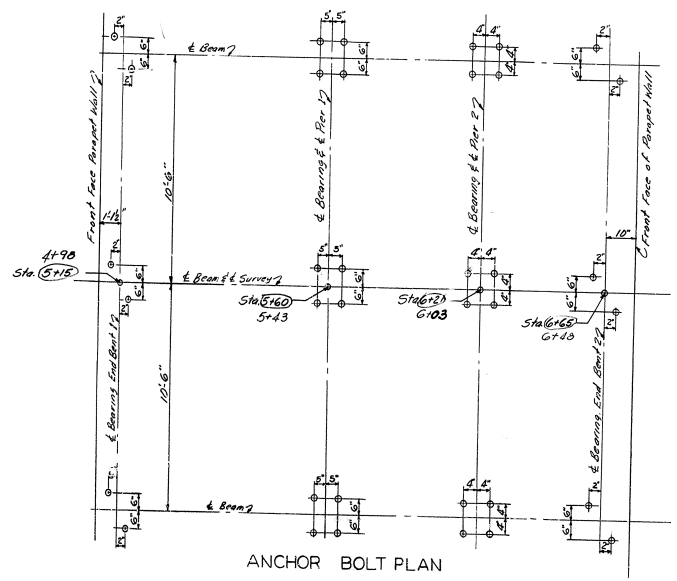
Std. Drwg. P. 2 (sheet)

**NOTE:**  
 This pile record does not replace other records of piles required to be kept and submitted by the Resident Engineer. After all piles have been driven, the Engineer shall record the tip of pile elevation as driven, length of pile in place, and calculate the bearing capacity of each pile, and return one blue print copy of this sheet with this data to the Bridge Engineer to be recorded on original plans. Length of piles in place shown hereon are the actual length of piles in the finished structure below cutoff elevation and is not necessarily the pay item.

Bridge over I-63, Sheet 3 A  
**COMMONWEALTH OF KENTUCKY**  
 DEPARTMENT OF HIGHWAYS  
 FRANKFORT  
 COUNTY OF  
**UNION**  
 UNIONTOWN - SPRING GROVE  
 ROAD  
 STATION 5+30 PROJECT NO. S-345 (2)  
 BRIDGE NUMBER \_\_\_\_\_ DRAWING NO. 12550



DESIGNED BY: *PC* CHECKED BY: *U.R.D.* DATE: *1/27/52*  
 DRAWN BY: *CEB* CHECKED BY: *U.R.D.* DATE: *1/27/52*  
 REVISION: *Station Change* DATE: *1/27/52*  
 REVISION: *CEB* DATE: *1/27/52*



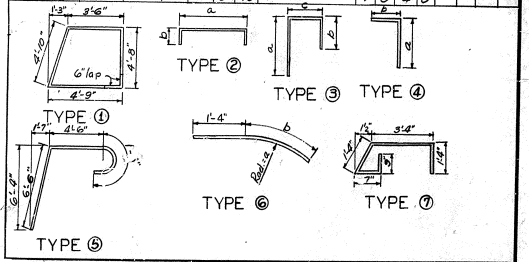
ANCHOR BOLT PLAN

**DRILLING ANCHOR BOLT HOLES & PLACING BOLTS**  
 Anchor bolt holes to be drilled to the size and depth shown on sheet 11. The contractor will be responsible for keeping the holes dry in freezing weather and will be required to repair any damage due to freezing at his own expense. After the shoes and anchor bolts are in place, the holes are to be completely filled to the top of base plate with molten lead. The bolts shall be protected to insure complete filling of holes. The cost of drilling holes, placing bolts, furnishing and filling holes with lead is to be included in the unit price bid for class "B" concrete. No direct payment will be made for furnishing lead and labor for this work.

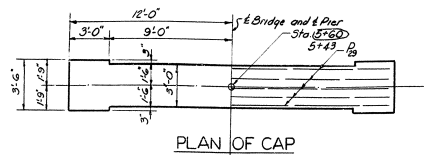
END BENT 1 AND 2. (Sheet 2)

| FIG. NO. | DATE | REV. | BY | CHK. | TOTAL |
|----------|------|------|----|------|-------|
| 7        | KY.  |      |    |      | 1     |

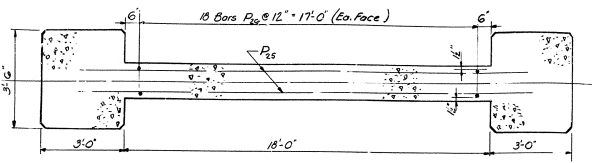
| MARK | TYPE | NUMBER |        |       | SIZE | LENGTH<br>FT./IN. | LOCATION          | DIMENSIONS |     |      |   |  |  |
|------|------|--------|--------|-------|------|-------------------|-------------------|------------|-----|------|---|--|--|
|      |      | E Bent | A Bent | Total |      |                   |                   | a          | b   | c    | d |  |  |
| A1   | (1)  | 30     | 30     | 60    | #5   | 18 5              | Cap               |            |     |      |   |  |  |
| A2   | Str  | 29     | 29     | 58    | #3   | 2                 | Cap & Parapet     |            |     |      |   |  |  |
| A3   | (2)  | 18     | 18     | 36    | #5   | 0                 | Cap               | 4 3        | 0 6 |      |   |  |  |
| A4   | "    | 18     | 18     | 36    | #5   | 4                 | "                 | 4 58       | 0 6 |      |   |  |  |
| A5   | (3)  | 45     | 45     | 90    | #7   | 7                 | Parapet           | 7 0        | 1 0 | 0 9  |   |  |  |
| A6   | (4)  | 67     | 67     | 134   | #6   | 11                | "                 | 6 0        | 1 0 |      |   |  |  |
| A7   | "    | 8      | 8      | 16    | #5   | 11                | Key               | 3 6        | 2 6 |      |   |  |  |
| A8   | Str  | 10     | 10     | 20    | #3   | 9                 | "                 |            |     |      |   |  |  |
| A9   | (5)  | 6      | 6      | 12    | #8   | 12 5              | Wall & End of Cap |            |     |      |   |  |  |
| A10  | Str  | 2      | 2      | 4     | #6   | 8 6               | Wing Embed. B.F.  |            |     |      |   |  |  |
| A11  | "    | 2      | 2      | 4     | #7   | 8                 | "                 |            |     |      |   |  |  |
| A12  | "    | 2      | 2      | 4     | #6   | 10                | "                 |            |     |      |   |  |  |
| A13  | "    | 2      | 2      | 4     | #6   | 0                 | "                 |            |     |      |   |  |  |
| A14  | "    | 2      | 2      | 4     | #5   | 2                 | "                 |            |     |      |   |  |  |
| A15  | "    | 2      | 2      | 4     | #4   | 4                 | "                 |            |     |      |   |  |  |
| A16  | "    | 2      | 2      | 4     | #5   | 6                 | "                 |            |     |      |   |  |  |
| A17  | "    | 2      | 2      | 4     | #5   | 8                 | "                 |            |     |      |   |  |  |
| A18  | "    | 2      | 2      | 4     | #7   | 8                 | "                 |            |     |      |   |  |  |
| A19  | "    | 2      | 2      | 4     | #6   | 10                | "                 |            |     |      |   |  |  |
| A20  | "    | 2      | 2      | 4     | #6   | 0                 | "                 |            |     |      |   |  |  |
| A21  | "    | 2      | 2      | 4     | #5   | 2                 | "                 |            |     |      |   |  |  |
| A22  | "    | 2      | 2      | 4     | #4   | 4                 | "                 |            |     |      |   |  |  |
| A23  | "    | 2      | 2      | 4     | #3   | 6                 | "                 |            |     |      |   |  |  |
| A24  | (6)  | 8      | 8      | 16    | #8   | 4                 | "                 | B.F.       | 7 0 | 7 0  |   |  |  |
| A25  | "    | 2      | 2      | 4     | #7   | 4                 | "                 | "          | 7 0 | 6 0  |   |  |  |
| A26  | "    | 2      | 2      | 4     | #6   | 0                 | "                 | "          | 7 0 | 4 8  |   |  |  |
| A27  | "    | 2      | 2      | 4     | #8   | 8                 | "                 | "          | 7 0 | 3 4  |   |  |  |
| A28  | "    | 8      | 8      | 16    | #5   | 7 4               | "                 | "          | 6 2 | 6 0  |   |  |  |
| A29  | "    | 2      | 2      | 4     | #6   | 5                 | "                 | "          | 6 2 | 5 1  |   |  |  |
| A30  | "    | 2      | 2      | 4     | #5   | 3                 | "                 | "          | 6 2 | 3 11 |   |  |  |
| A31  | "    | 2      | 2      | 4     | #4   | 1                 | "                 | "          | 6 2 | 2 9  |   |  |  |
| A32  | Str  | 4      | 4      | 8     | #9   | 6                 | Ball of Bracket   |            |     |      |   |  |  |
| A33  | (7)  | 16     | 16     | 32    | #7   | 6                 | Subwalk           | 9 4        | 6 2 |      |   |  |  |
| A34  | (8)  | 12     | 12     | 24    | #7   | 0                 | "                 |            |     |      |   |  |  |
| A35  | "    | 2      | 2      | 4     | #3   | 3                 | "                 |            |     |      |   |  |  |
| A36  | (3)  | 18     | 18     | 36    | #7   | 9                 | Roll              | 2 0        | 1 4 |      |   |  |  |
| A37  | (6)  | 6      | 6      | 12    | #4   | 4                 | "                 | 6 10       | 7 0 |      |   |  |  |
| A38  | "    | 6      | 6      | 12    | #7   | 7                 | "                 | 6 10       | 7 0 |      |   |  |  |
| A39  | (2)  | 8      | 8      | 16    | #8   | 10                | Bracket           | 1 0        | 4 0 |      |   |  |  |



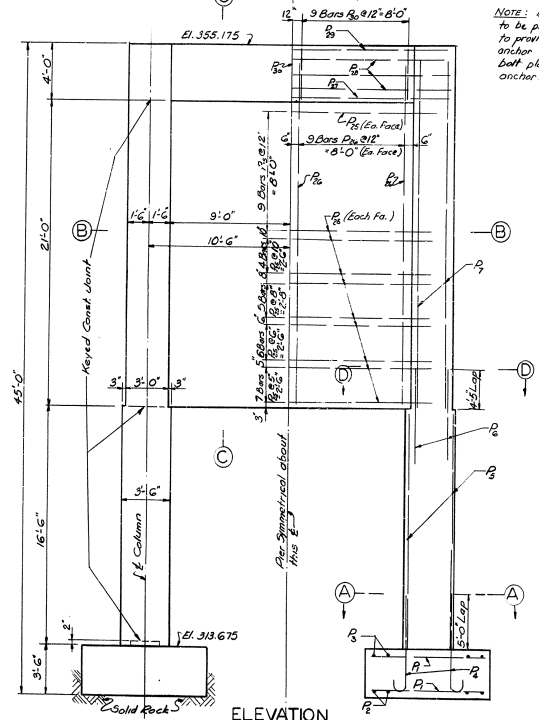
Bridge Over Lost Creek Sheet 4  
**COMMONWEALTH OF KENTUCKY**  
 DEPARTMENT OF HIGHWAYS  
 FRANKFORT  
 COUNTY OF  
 UNION  
 UNIONTOWN - SPRING GROVE  
 ROAD  
 STATION 5+33  
 BRIDGE NUMBER PROJECT NO. S-345 (2)  
 DRAWN BY: *U.R.D.* DATE: *1/27/52*  
 CHECKED BY: *U.R.D.* DATE: *1/27/52*



PLAN OF CAP

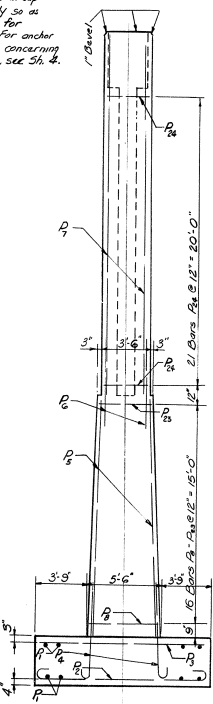


SECTION B-B

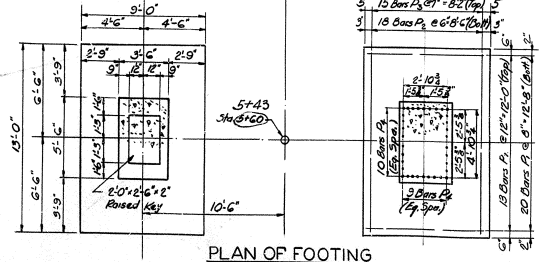


ELEVATION

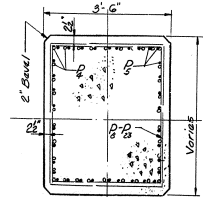
NOTE: Bars P23 & P24 in cap to be placed carefully so as to provide clearance for unlap bull hooks. For anchor bolt plan and notes concerning anchor bolt & holes, see Sh. 4.



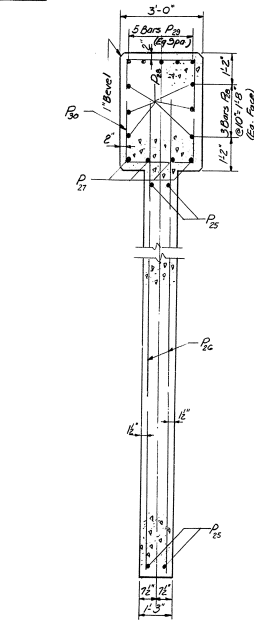
END ELEVATION



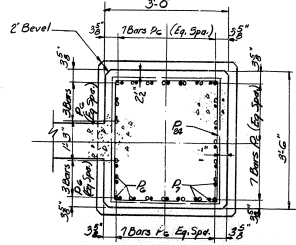
PLAN OF FOOTING



SECTION A-A

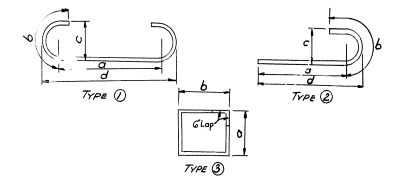


SECTION C-C



SECTION D-D

| BILL OF REINFORCEMENT |      |     |      |        |                     |        |     |     |   |       |        |     |     |
|-----------------------|------|-----|------|--------|---------------------|--------|-----|-----|---|-------|--------|-----|-----|
| MARK                  | TYPE | NO. | SIZE | LENGTH | LOCATION            | Q      | FR. | IN. | b | FR.   | IN.    | FR. | IN. |
| P1                    | Str. | 66  | 3    | 9      | Top Bull of Footing | 12     | 2   | 1   | 2 | 0     | 7      | 12  | 9   |
| P2                    | ①    | 36  | 7    | 14     | Bot. of Footing     |        |     |     |   |       |        |     |     |
| P3                    | Str. | 30  | 5    | 12     | Top "               |        |     |     |   |       |        |     |     |
| P4                    | ②    | 68  | 9    | 9      | Column Dowels       | 7      | 3   | 1   | 8 | 0     | 11 1/2 | 8   | 2   |
| P5                    | Str. | 60  | 9    | 16     | Columns             |        |     |     |   |       |        |     |     |
| P6                    | Str. | 46  | 8    | 8      | Column Dowels       |        |     |     |   |       |        |     |     |
| P7                    | Str. | 46  | 8    | 24     | Columns             |        |     |     |   |       |        |     |     |
| P8                    | ③    | 2   | 4    | 17     | 0                   | (Tras) | 3   | 1   | 5 | 0     |        |     |     |
| P9                    | "    | 2   | "    | 16     | 9                   | "      | 3   | 1   | 4 | 11    |        |     |     |
| P10                   | "    | 2   | "    | 16     | 7                   | "      | 3   | 1   | 4 | 10    |        |     |     |
| P11                   | "    | 2   | "    | 16     | 5                   | "      | 3   | 1   | 4 | 8 1/2 |        |     |     |
| P12                   | "    | 2   | "    | 16     | 3                   | "      | 3   | 1   | 4 | 7 1/2 |        |     |     |
| P13                   | "    | 2   | "    | 16     | 0                   | "      | 3   | 1   | 4 | 6 1/2 |        |     |     |
| P14                   | "    | 2   | "    | 15     | 10                  | "      | 3   | 1   | 4 | 5 1/2 |        |     |     |
| P15                   | "    | 2   | "    | 15     | 8                   | "      | 3   | 1   | 4 | 4 1/2 |        |     |     |
| P16                   | "    | 2   | "    | 15     | 6                   | "      | 3   | 1   | 4 | 3 1/2 |        |     |     |
| P17                   | "    | 2   | "    | 15     | 4                   | "      | 5   | 1   | 4 | 2 1/2 |        |     |     |
| P18                   | "    | 2   | "    | 15     | 1                   | "      | 3   | 1   | 4 | 1 1/2 |        |     |     |
| P19                   | "    | 2   | "    | 14     | 11                  | "      | 3   | 1   | 4 | 0     |        |     |     |
| P20                   | "    | 2   | "    | 14     | 9                   | "      | 3   | 1   | 3 | 11    |        |     |     |
| P21                   | "    | 2   | "    | 14     | 7                   | "      | 3   | 1   | 3 | 9 1/2 |        |     |     |
| P22                   | "    | 2   | "    | 14     | 4                   | "      | 3   | 1   | 3 | 7 1/2 |        |     |     |
| P23                   | "    | 2   | "    | 14     | 1                   | "      | 3   | 1   | 3 | 5 1/2 |        |     |     |
| P24                   | "    | 42  | "    | 12     | 1                   | "      | 2   | 7   | 3 | 1     |        |     |     |
| P25                   | Str. | 62  | 5    | 21     | 0                   | Wall   |     |     |   |       |        |     |     |
| P26                   | "    | 33  | 4    | 22     | 6                   | "      |     |     |   |       |        |     |     |
| P27                   | "    | 4   | 9    | 23     | 9                   | Cap    |     |     |   |       |        |     |     |
| P28                   | "    | 6   | 6    | 23     | 9                   | "      |     |     |   |       |        |     |     |
| P29                   | "    | 5   | 9    | 23     | 9                   | "      |     |     |   |       |        |     |     |
| P30                   | ④    | 19  | 8    | 13     | 5                   | "      | 2   | 8   | 3 | 8     |        |     |     |



ESTIMATE OF QUANTITIES

Concrete, Class X 956 Cu. Yds.  
Reinforcement 15833 Lbs.

Bridge over Lost Creek Sheet 5

**COMMONWEALTH OF KENTUCKY**  
DEPARTMENT OF HIGHWAYS  
FRANKFORT  
COUNTY OF  
UNION  
UNIONTOWN-SPRING GROVE

PIER NO. 1

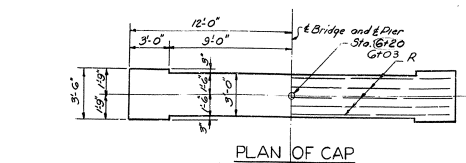
STATION 5+73  
BRIDGE NUMBER 5730

ROAD PROJECT NO. S 345 (2)

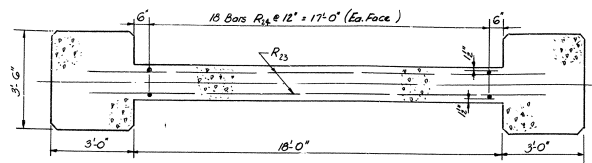
DATE 12/20

DESIGNED BY: [Name] CHECKED BY: [Name] DATE: [Date]  
DRAWN BY: [Name] CHECKED BY: [Name] DATE: [Date]  
SCALE: [Scale]

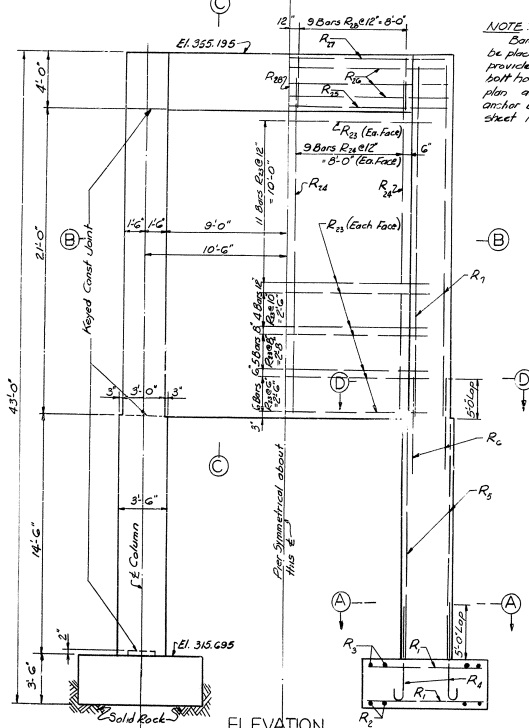
DRAWN BY: J.E.D. CHECKED BY: C.E.B. DATE: 7/1/50  
 REVISIONS: 1. REVISION 2. REVISION 3. REVISION  
 APPROVED BY: J.E.D. DATE: 7/1/50



PLAN OF CAP

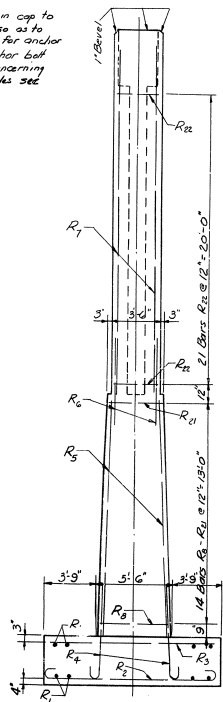


SECTION B-B

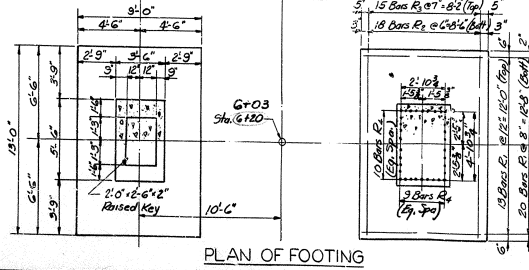


ELEVATION

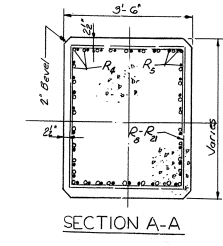
NOTE:  
 Bars R13 & R14 in cap to be placed carefully so as to provide clearance for anchor bolt holes. For anchor bolt plan and notes concerning anchor bolts and holes see sheet No. 4.



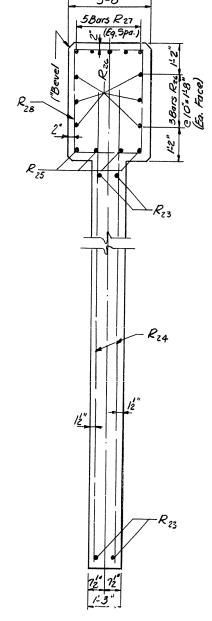
END ELEVATION



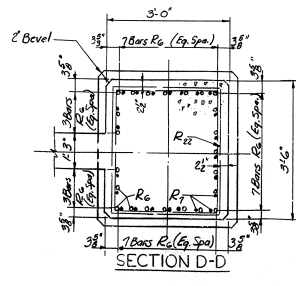
PLAN OF FOOTING



SECTION A-A

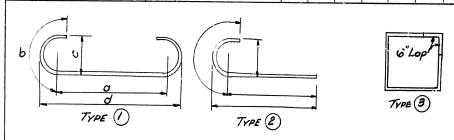


SECTION C-C



SECTION D-D

| BILL OF REINFORCEMENT |      |     |      |        |        |                 |     |     |     |        |              |
|-----------------------|------|-----|------|--------|--------|-----------------|-----|-----|-----|--------|--------------|
| Max                   | Type | No. | Size | Length | Weight | Location        | a   | b   | c   | d      |              |
|                       |      |     |      | FT.    | LB.    |                 | IN. | IN. | IN. | IN.    |              |
| R1                    | Sfr  | 66  | *5   | 2      | 3      | Top of Footing  |     |     |     |        |              |
| R2                    | ①    | 36  | 7    | 14     | 6      | Both of Footing | 12  | 2   | 1   | 2      | 0 7 12 9     |
| R3                    | Sfr  | 30  | 5    | 12     | 9      | Top of Footing  |     |     |     |        |              |
| R4                    | ②    | 68  | 9    | 3      | 5      | Column Dowels   | 7   | 9   | 1   | 8      | 0 11 1/2 1 2 |
| R5                    | Sfr  | 68  | 9    | 14     | 4      | Columns         |     |     |     |        |              |
| R6                    | Sfr  | 46  | 9    | 10     | 0      | Column Dowels   |     |     |     |        |              |
| R7                    | Sfr  | 46  | 9    | 24     | 6      | Columns         |     |     |     |        |              |
| R8                    | ③    | 2   | 4    | 16     | 11     | - Bars          | 3   | 1   | 5   | 0      |              |
| R9                    | "    | 2   | "    | 10     | 9      | "               | 3   | 1   | 4   | 10 1/2 |              |
| R10                   | "    | 2   | "    | 16     | 6      | "               | 3   | 1   | 4   | 9 1/2  |              |
| R11                   | "    | 2   | "    | 16     | 4      | "               | 3   | 1   | 4   | 7      |              |
| R12                   | "    | 2   | "    | 16     | 1      | "               | 3   | 1   | 4   | 7      |              |
| R13                   | "    | 2   | "    | 15     | 11     | "               | 3   | 1   | 4   | 5 1/2  |              |
| R14                   | "    | 2   | "    | 15     | 8      | "               | 3   | 1   | 4   | 4 1/2  |              |
| R15                   | "    | 2   | "    | 15     | 6      | "               | 3   | 1   | 4   | 3 1/2  |              |
| R16                   | "    | 2   | "    | 15     | 3      | "               | 3   | 1   | 4   | 2      |              |
| R17                   | "    | 2   | "    | 15     | 1      | "               | 3   | 1   | 4   | 1      |              |
| R18                   | "    | 2   | "    | 14     | 11     | "               | 3   | 1   | 3   | 11 1/2 |              |
| R19                   | "    | 2   | "    | 14     | 8      | "               | 3   | 1   | 3   | 10 1/2 |              |
| R20                   | "    | 2   | "    | 14     | 3      | "               | 3   | 1   | 3   | 8 1/2  |              |
| R21                   | "    | 2   | "    | 14     | 3      | "               | 3   | 1   | 3   | 8      |              |
| R22                   | "    | 42  | "    | 12     | 1      | "               | "   | "   | "   | "      |              |
| R23                   | Sfr  | 52  | 5    | 21     | 6      | Wall            | 2   | 7   | 3   | 1      |              |
| R24                   | "    | 36  | 4    | 22     | 6      | "               | "   | "   | "   | "      |              |
| R25                   | "    | 4   | 9    | 23     | 9      | Cap             | "   | "   | "   | "      |              |
| R26                   | "    | 6   | 6    | 23     | 9      | "               | "   | "   | "   | "      |              |
| R27                   | "    | 5   | 9    | 23     | 9      | "               | "   | "   | "   | "      |              |
| R28                   | ③    | 19  | 5    | 13     | 5      | "               | 2   | 8   | 3   | 8      |              |



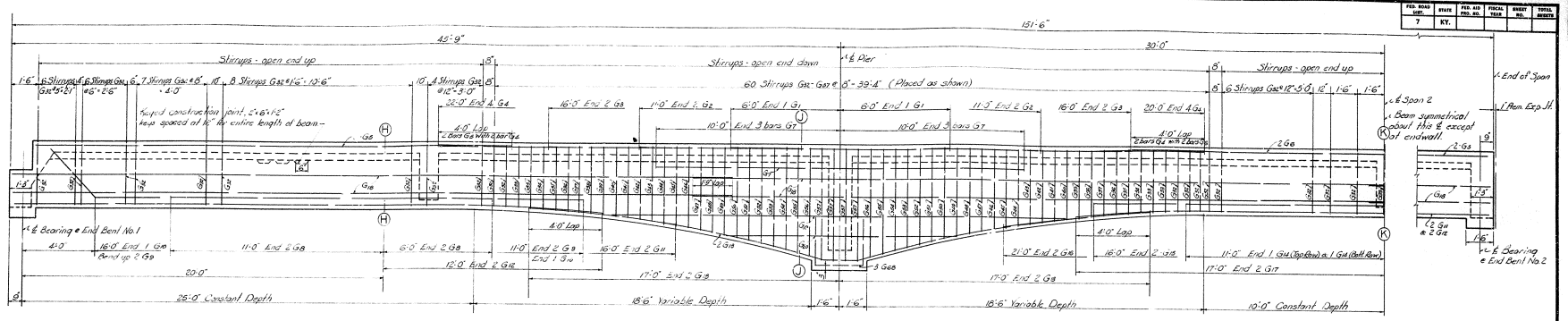
ESTIMATE OF QUANTITIES

Concrete, Class 71 93.1 Cu. Yds.  
 Reinforcement 15500. Lbs.

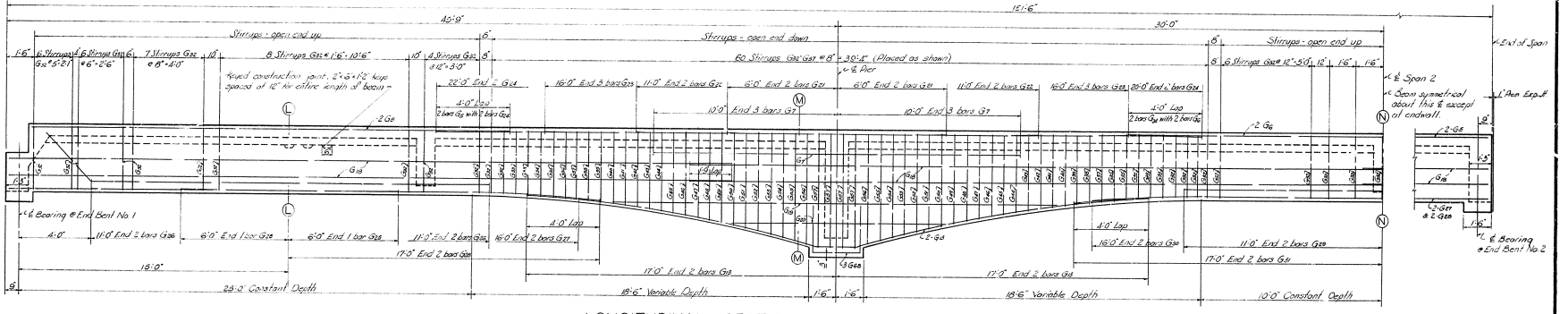
BRIDGE OVER LEAS CREEK Sheet 6  
**COMMONWEALTH OF KENTUCKY**  
 DEPARTMENT OF HIGHWAYS  
 FRANKFORT COUNTY OF  
 UNION  
 UNIONTOWN - SPRING GROVE  
 ROAD  
 STATION 5173  
 BRIDGE NUMBER 5190 PROJECT NO. 5345(2)  
 DRAWING INDEX NO. 12520

PIER NO. 2

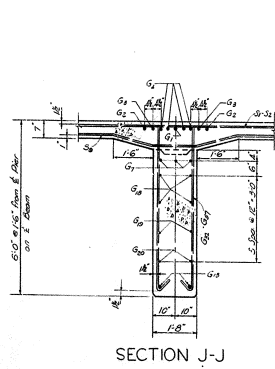




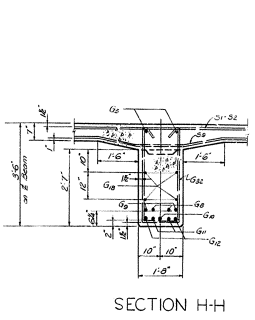
LONGITUDINAL SECTION - INTERIOR BEAM



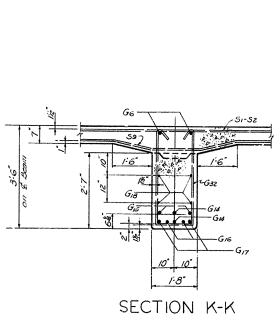
LONGITUDINAL SECTION - EXTERIOR BEAM



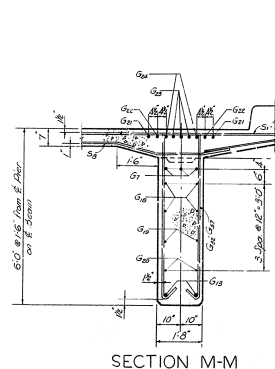
SECTION J-J



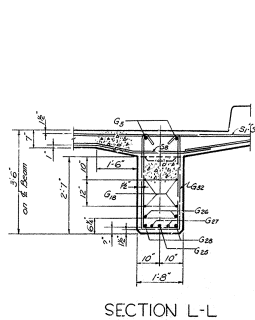
SECTION H-H



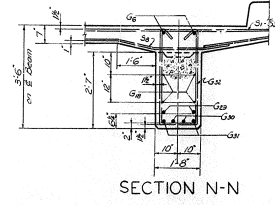
SECTION K-K



SECTION M-M



SECTION L-L



SECTION N-N

BEAMS

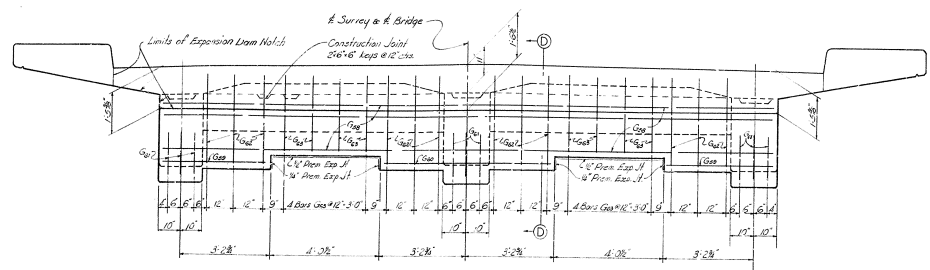
DESIGNED BY: [Signature] CHECKED BY: [Signature] DATE: [Date]  
 DRAWN BY: [Signature] REVISION: [Signature] DATE: [Date]

BRIDGE OVER LOST CREEK SHEET NO. 8

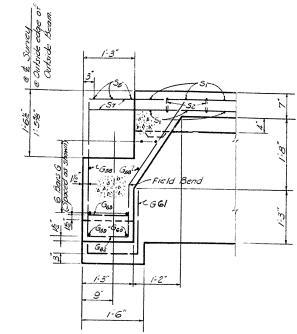
**COMMONWEALTH OF KENTUCKY**  
 DEPARTMENT OF HIGHWAYS  
 TRANSPORTATION COUNTY OF  
**UNION**  
 UNIONTOWN - SPRING GROVE ROAD

STATION 5+73  
 BRIDGE NUMBER 9-1-99 PROJECT NO. [Blank]  
 DRAWING NO. 12520

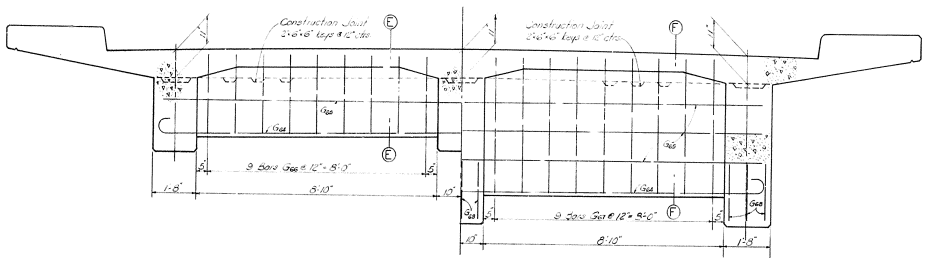
| REV. NO. | DATE | BY | CHKD. | REASON |
|----------|------|----|-------|--------|
| 7        | KT.  |    |       |        |



END ELEVATION B-B  
(SHOWING REINFORCEMENT IN ENDWALL)  
AT END BENT NO 1

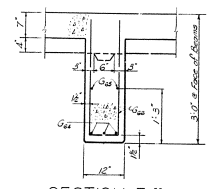


SECTION D-D

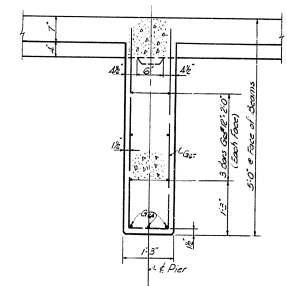


SECTION P-P  
(SHOWING REINFORCEMENT IN  
INTERMEDIATE DIAPHRAGM)

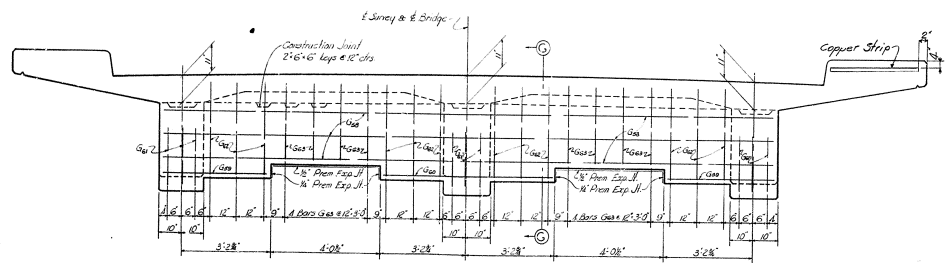
SECTION R-R  
(SHOWING REINFORCEMENT IN  
DIAPHRAGM OVER PIERS)



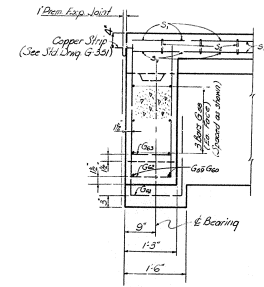
SECTION E-E



SECTION F-F



END ELEVATION C-C  
(SHOWING REINFORCEMENT IN ENDWALL)  
AT END BENT NO 2



SECTION G-G

DRAWN BY: [Signature] CHECKED BY: [Signature] DATE: 5/21/73  
 DESIGNED BY: [Signature] CHECKED BY: [Signature] DATE: 5/21/73  
 PROJECT NO. 5173  
 STATION 5+73  
 BRIDGE NUMBER 12520

DIAPHRAGMS AND ENDWALLS

BRIDGE OVER LOST CREEK SHEET NO. 9

**COMMONWEALTH OF KENTUCKY**  
 DEPARTMENT OF HIGHWAYS  
 FRANKFORT  
 COUNTY OF  
 UNION  
 UNIONTOWN - SPRING GROVE  
 ROAD

STATION 5+73 PROJECT NO. 12520  
 BRIDGE NUMBER 12520 DRAWING NO. 12520

**BILL OF REINFORCEMENT**

| MARK  | TYPE | NO  | SIZE | LENGTH<br>FT. IN. | LOCATION | SPAN 1 |     | SPAN 2 |     | SPAN 3 |     |
|-------|------|-----|------|-------------------|----------|--------|-----|--------|-----|--------|-----|
|       |      |     |      |                   |          | FT.    | IN. | FT.    | IN. | FT.    | IN. |
| S.1   | Ⓟ    | 164 | 5    | 28                | 6        |        |     |        |     |        |     |
| S.2   | Ⓟ    | 165 | 5    | 29                | 0        |        |     |        |     |        |     |
| S.3   | Ⓟ    | 166 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.4   | Ⓟ    | 167 | 5    | 5                 | 3        |        |     |        |     |        |     |
| S.5   | Ⓟ    | 168 | 5    | 12                | 5        |        |     |        |     |        |     |
| S.6   | Ⓟ    | 169 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.7   | Ⓟ    | 170 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.8   | Ⓟ    | 171 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.9   | Ⓟ    | 172 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.10  | Ⓟ    | 173 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.11  | Ⓟ    | 174 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.12  | Ⓟ    | 175 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.13  | Ⓟ    | 176 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.14  | Ⓟ    | 177 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.15  | Ⓟ    | 178 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.16  | Ⓟ    | 179 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.17  | Ⓟ    | 180 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.18  | Ⓟ    | 181 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.19  | Ⓟ    | 182 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.20  | Ⓟ    | 183 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.21  | Ⓟ    | 184 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.22  | Ⓟ    | 185 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.23  | Ⓟ    | 186 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.24  | Ⓟ    | 187 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.25  | Ⓟ    | 188 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.26  | Ⓟ    | 189 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.27  | Ⓟ    | 190 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.28  | Ⓟ    | 191 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.29  | Ⓟ    | 192 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.30  | Ⓟ    | 193 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.31  | Ⓟ    | 194 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.32  | Ⓟ    | 195 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.33  | Ⓟ    | 196 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.34  | Ⓟ    | 197 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.35  | Ⓟ    | 198 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.36  | Ⓟ    | 199 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.37  | Ⓟ    | 200 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.38  | Ⓟ    | 201 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.39  | Ⓟ    | 202 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.40  | Ⓟ    | 203 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.41  | Ⓟ    | 204 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.42  | Ⓟ    | 205 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.43  | Ⓟ    | 206 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.44  | Ⓟ    | 207 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.45  | Ⓟ    | 208 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.46  | Ⓟ    | 209 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.47  | Ⓟ    | 210 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.48  | Ⓟ    | 211 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.49  | Ⓟ    | 212 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.50  | Ⓟ    | 213 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.51  | Ⓟ    | 214 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.52  | Ⓟ    | 215 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.53  | Ⓟ    | 216 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.54  | Ⓟ    | 217 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.55  | Ⓟ    | 218 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.56  | Ⓟ    | 219 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.57  | Ⓟ    | 220 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.58  | Ⓟ    | 221 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.59  | Ⓟ    | 222 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.60  | Ⓟ    | 223 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.61  | Ⓟ    | 224 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.62  | Ⓟ    | 225 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.63  | Ⓟ    | 226 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.64  | Ⓟ    | 227 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.65  | Ⓟ    | 228 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.66  | Ⓟ    | 229 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.67  | Ⓟ    | 230 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.68  | Ⓟ    | 231 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.69  | Ⓟ    | 232 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.70  | Ⓟ    | 233 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.71  | Ⓟ    | 234 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.72  | Ⓟ    | 235 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.73  | Ⓟ    | 236 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.74  | Ⓟ    | 237 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.75  | Ⓟ    | 238 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.76  | Ⓟ    | 239 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.77  | Ⓟ    | 240 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.78  | Ⓟ    | 241 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.79  | Ⓟ    | 242 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.80  | Ⓟ    | 243 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.81  | Ⓟ    | 244 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.82  | Ⓟ    | 245 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.83  | Ⓟ    | 246 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.84  | Ⓟ    | 247 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.85  | Ⓟ    | 248 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.86  | Ⓟ    | 249 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.87  | Ⓟ    | 250 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.88  | Ⓟ    | 251 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.89  | Ⓟ    | 252 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.90  | Ⓟ    | 253 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.91  | Ⓟ    | 254 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.92  | Ⓟ    | 255 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.93  | Ⓟ    | 256 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.94  | Ⓟ    | 257 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.95  | Ⓟ    | 258 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.96  | Ⓟ    | 259 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.97  | Ⓟ    | 260 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.98  | Ⓟ    | 261 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.99  | Ⓟ    | 262 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.100 | Ⓟ    | 263 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.101 | Ⓟ    | 264 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.102 | Ⓟ    | 265 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.103 | Ⓟ    | 266 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.104 | Ⓟ    | 267 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.105 | Ⓟ    | 268 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.106 | Ⓟ    | 269 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.107 | Ⓟ    | 270 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.108 | Ⓟ    | 271 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.109 | Ⓟ    | 272 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.110 | Ⓟ    | 273 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.111 | Ⓟ    | 274 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.112 | Ⓟ    | 275 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.113 | Ⓟ    | 276 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.114 | Ⓟ    | 277 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.115 | Ⓟ    | 278 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.116 | Ⓟ    | 279 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.117 | Ⓟ    | 280 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.118 | Ⓟ    | 281 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.119 | Ⓟ    | 282 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.120 | Ⓟ    | 283 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.121 | Ⓟ    | 284 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.122 | Ⓟ    | 285 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.123 | Ⓟ    | 286 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.124 | Ⓟ    | 287 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.125 | Ⓟ    | 288 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.126 | Ⓟ    | 289 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.127 | Ⓟ    | 290 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.128 | Ⓟ    | 291 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.129 | Ⓟ    | 292 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.130 | Ⓟ    | 293 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.131 | Ⓟ    | 294 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.132 | Ⓟ    | 295 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.133 | Ⓟ    | 296 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.134 | Ⓟ    | 297 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.135 | Ⓟ    | 298 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.136 | Ⓟ    | 299 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.137 | Ⓟ    | 300 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.138 | Ⓟ    | 301 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.139 | Ⓟ    | 302 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.140 | Ⓟ    | 303 | 5    | 11                | 9        |        |     |        |     |        |     |
| S.141 | Ⓟ    | 304 | 5    | 11                | 9        |        |     |        |     |        |     |

| NO. | DATE | BY | CHKD. | SCALE | SHEET | TOTAL |
|-----|------|----|-------|-------|-------|-------|
| 7   | KY.  |    |       |       |       |       |

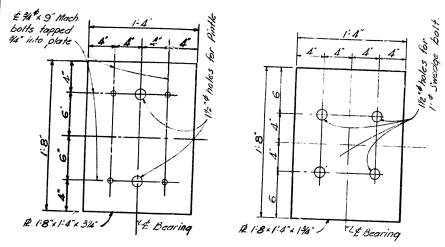
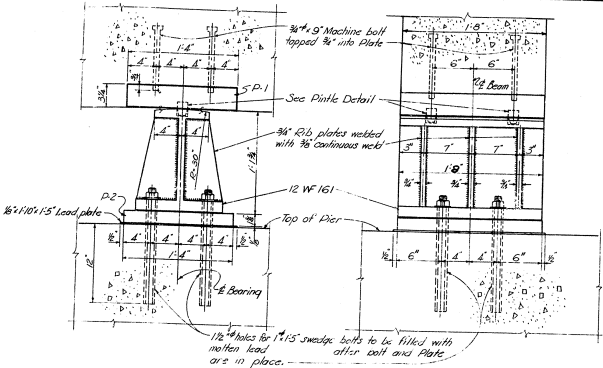


PLATE P-1  
PLATE P-2



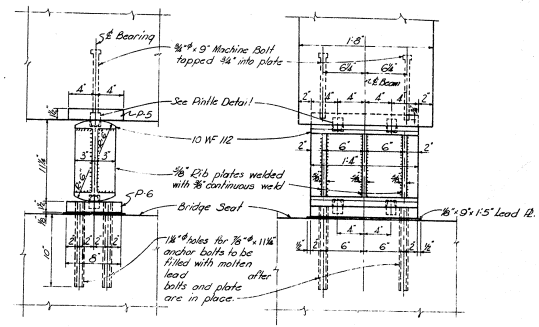
END ELEVATION  
SIDE ELEVATION

**FIXED SHOE ASSEMBLY AT PIER NO. 2**

| Qty | Part          | Required | Weight, Each | Total    |
|-----|---------------|----------|--------------|----------|
| 3   | Bottom Plates | P-2      | 155 Lbs.     | 465 Lbs. |
| 3   | Top Plates    | P-1      | 300 Lbs.     | 900 Lbs. |
| 3   | Fixed Shoes   | -        | 290 Lbs.     | 870 Lbs. |
| 12  | Sledge Bolts  | -        | 4 Lbs.       | 48 Lbs.  |
| 3   | Lead Plates   | -        | 18 Lbs.      | 54 Lbs.  |

\* Includes Pintles and Machine Bolts.

**FACING OF BEARING SURFACES.**  
Heavy plates in contact in shoes to be welded - Am. Sls. Ass'n 1000  
Bridge rockers, and Brq. Pls. in contact with rollers " " " 250



END ELEVATION  
SIDE ELEVATION

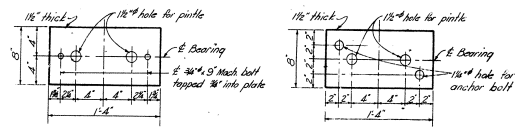
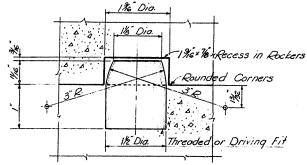


PLATE P-5  
PLATE P-6

**EXPANSION ROCKER AT ABUTMENTS NO. 1 AND NO. 2**

| Qty | Part          | Required | Weight, Each | Total    |
|-----|---------------|----------|--------------|----------|
| 6   | Bottom Plates | P-6      | 54 Lbs.      | 324 Lbs. |
| 6   | Top Plates    | P-5      | 57 Lbs.      | 342 Lbs. |
| 6   | Rockers       | -        | 104 Lbs.     | 624 Lbs. |
| 12  | Anchor Bolts  | -        | 2 Lbs.       | 24 Lbs.  |
| 6   | Lead Plates   | -        | 9 Lbs.       | 54 Lbs.  |

\* Includes Pintles  
\* Includes Pintles and Machine Bolts.



DETAILS OF PINTLE AND RECESS

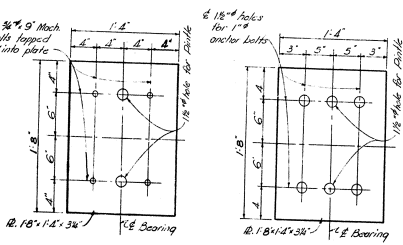
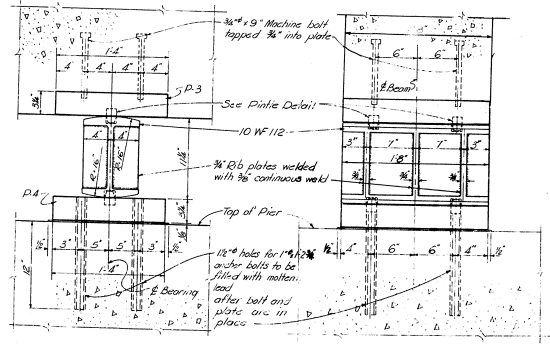


PLATE P-3  
PLATE P-4



END ELEVATION  
SIDE ELEVATION

**EXPANSION ROCKER AT PIERS NO. 1 AND NO. 2**

| Qty | Part          | Required | Weight, Each | Total     |
|-----|---------------|----------|--------------|-----------|
| 6   | Bottom Plates | P-6      | 289 Lbs.     | 1734 Lbs. |
| 6   | Top Plates    | P-5      | 300 Lbs.     | 1800 Lbs. |
| 6   | Rockers       | -        | 170 Lbs.     | 1020 Lbs. |
| 12  | Anchor Bolts  | -        | 33 Lbs.      | 396 Lbs.  |
| 6   | Lead Plates   | -        | 18 Lbs.      | 108 Lbs.  |

\* Includes Pintle  
\* Includes Pintle and Machine Bolts

**ALTERNATE SHOE AND ROCKER FABRICATION:** The contractor is permitted as an alternate to build up fixed shoes and expansion rockers from plates of same size used in flanges and webs of WF sections indicated on the plans. The fixed shoes and expansion rockers fabricated from plates are to be welded at plate contacts with 3/8" continuous welds.

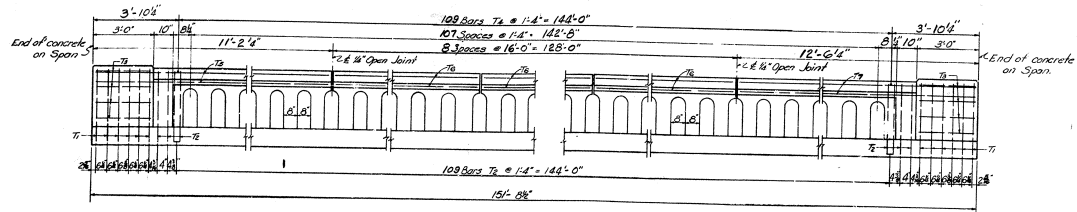
DRAWN BY: [Name] CHECKED BY: [Name] DATE: [Date]  
 DESIGNED BY: [Name] APPROVED BY: [Name] DATE: [Date]  
 REVISIONS: [List]

Bridge Over Lost Creek Sheet 11

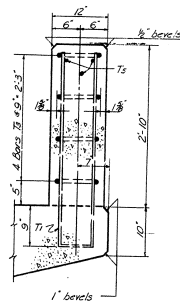
**COMMONWEALTH OF KENTUCKY**  
DEPARTMENT OF HIGHWAYS  
FRANKFORT  
COUNTY OF  
UNION  
UNIONTOWN - SPRING GROVE  
3+73 ROAD PROJECT NO. S-345 (2)  
STATION 5+30 BRIDGE NUMBER DRAWING NO. 12520



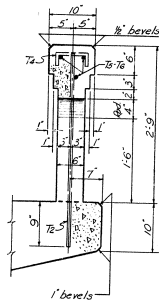




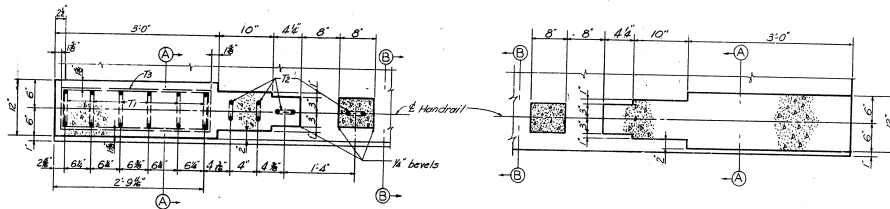
ELEVATION



SECTION A-A



SECTION B-B



PART SECTIONAL PLAN  
AT ABUTMENT NO. 1

PART SECTIONAL PLAN  
AT ABUTMENT NO. 2

(Reinforcement same as shown in Sectional Plan of Abut. No. 1)

| NO. DATE |     | DATE | REV. AND | FROM | YEAR | REVISION | BY | DATE |
|----------|-----|------|----------|------|------|----------|----|------|
| 7        | ST. |      |          |      |      |          |    |      |

| BILL OF REINFORCEMENT (TWO RAILS) |      |     |      |        |          |     |       |     |     |
|-----------------------------------|------|-----|------|--------|----------|-----|-------|-----|-----|
| MARK                              | TYPE | NO. | SIZE | LENGTH | LOCATION | FT. | IN.   | FT. | IN. |
| T <sub>1</sub>                    | 0    | 22  | 5    | 7      | Boards   | 3   | 5 1/2 | 0   | 8   |
| T <sub>2</sub>                    | 0    | 22  | 5    | 7      | Boards   | 3   | 5 1/2 | 0   | 8   |
| T <sub>3</sub>                    | 0    | 16  | 3    | 7      | Boards   | 3   | 5 1/2 | 0   | 8   |
| T <sub>4</sub>                    | 0    | 12  | 4    | 7      | Boards   | 3   | 5 1/2 | 0   | 8   |
| T <sub>5</sub>                    | 0    | 6   | 4    | 15     | Boards   | 3   | 5 1/2 | 0   | 8   |
| T <sub>6</sub>                    | 0    | 4   | 15   | 9      | Boards   | 3   | 5 1/2 | 0   | 8   |
| T <sub>7</sub>                    | 0    | 4   | 12   | 8      | Boards   | 3   | 5 1/2 | 0   | 8   |

| TYPE ① |  | TYPE ② |  |
|--------|--|--------|--|
|        |  |        |  |

ESTIMATE OF QUANTITIES

CONCRETE, CLASS "D" 13.7 CU. YDS.  
 REINFORCEMENT 2912 LBS.

DESIGNED BY: J.E.O. DATE: 9-25-11  
 CHECKED BY: J.E.O. DATE: 11-13-11  
 DRAWN BY: S. J. CHAPMAN DATE: 11-13-11  
 PROJECT NO. 5-345(2) SHEET NO. 3

BRIDGE OVER LOST CREEK SHEET NO. 3

**COMMONWEALTH OF KENTUCKY**  
 DEPARTMENT OF HIGHWAYS  
 FRANKFORT  
 COUNTY OF  
 UNION  
 UNIONTOWN - SPRING GROVE

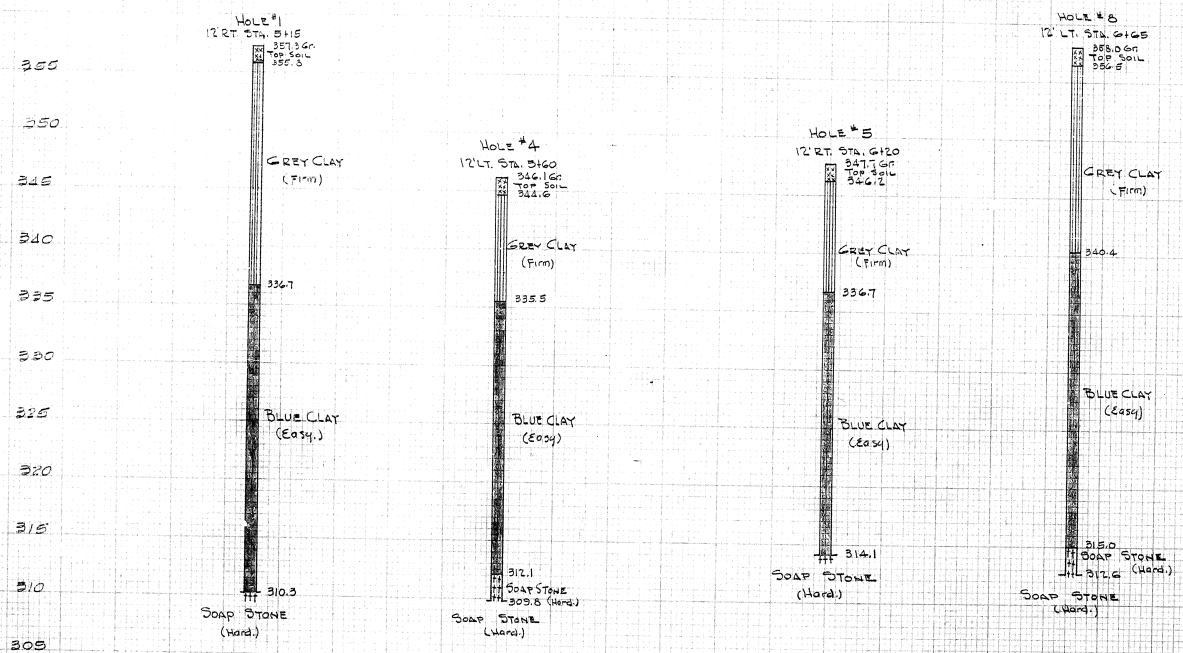
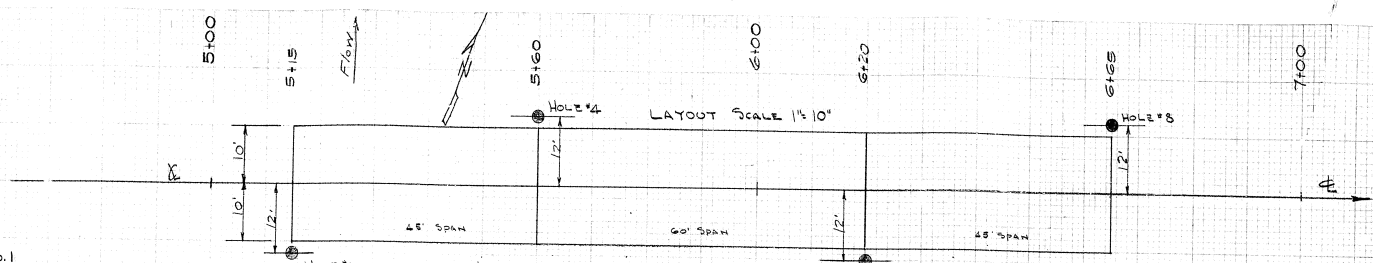
ROAD 5+73  
 STATION 5+30 PROJECT NO. 5-345(2)

BRIDGE NUMBER DRAWING NO. 12520

S. 15. 113. 287-41  
 UNIONTOWN - SPRING GROVE  
 ROAD

B.M. No. 1  
 U.S.G.S. - U.E. CHISEL & S.W. WINGARUT. NE. CORNER,  
 12' RT. STA. 615. ELEV. 337.89

SOUNDINGS FOR BRIDGE  
 ON THE UNIONTOWN - SPRING  
 GROVE ROAD -  
 45'-60'-45' SPANS



LEGEND FOR PLOTTING SOUNDINGS

- TOP SOIL
- GREY CLAY
- BLUE CLAY
- SOAP STONE

(SCALE FOR PLOTTING HOLE - 1/4" = 1')

PLATE 3 CROSS SECTION C, R, & C STANDARD  
 300-28700P YELLOW PAPER AND PRINTED IN U.S.A.

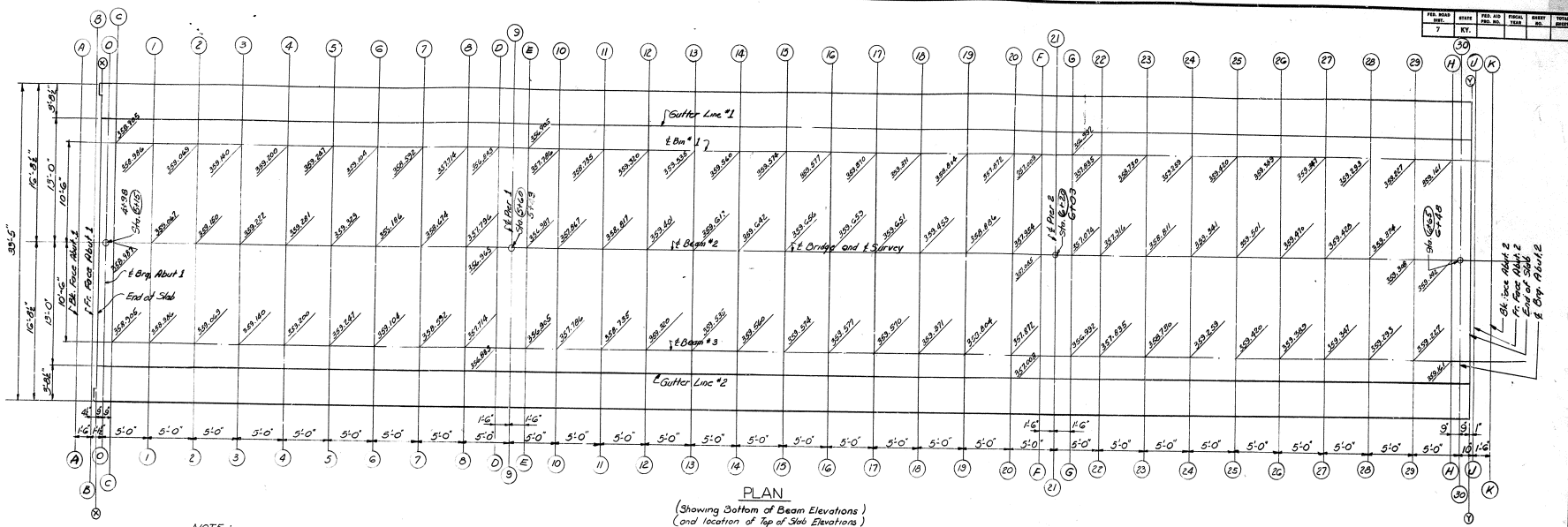
Bridge Over East Creek Sheet 15  
 COMMONWEALTH OF KENTUCKY  
 DEPARTMENT OF HIGHWAYS  
 FRANKLIN COUNTY OF  
 UNION  
 UNIONTOWN - SPRING GROVE  
 ROAD  
 STATION 5+90 PROJECT NO. S-345 (8)  
 DRAWING 12320

FINAL SERVICE CHECKED BY DATE

Revised Station Changes 02/8/76

ORIGINAL DRAWING BY DATE

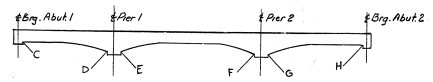
DESIGNED BY: J.R.O. DATE: 12/15/58  
 CHECKED BY: C.E.B. DATE: 1/10/59  
 DRAWN BY: J.R.O. DATE: 12/15/58  
 REVISIONS:



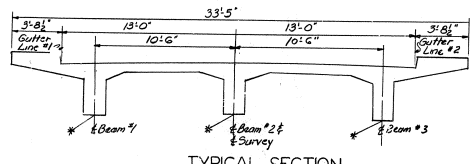
PLAN  
 (Showing Bottom of Beam Elevations  
 and location of Top of Slab Elevations)

NOTE:  
 Elevations shown herein contain  
 construction camber and are to be  
 maintained with falsework in place.

| Station | TOP OF SLAB ELEVATIONS |            |            | Gutter<br>Line #2<br>Lm. #2 |
|---------|------------------------|------------|------------|-----------------------------|
|         | Gutter<br>Line #1      | Beam<br>#1 | Beam<br>#2 |                             |
| A-A     | 362.310                | 362.354    | 362.355    | 362.310                     |
| B-B     | 353                    | 371        | 452        | 371                         |
| C-C     | 353                    | 383        | 462        | 383                         |
| D-D     | 353                    | 385        | 462        | 385                         |
| E-E     | 353                    | 385        | 462        | 385                         |
| F-F     | 353                    | 385        | 462        | 385                         |
| G-G     | 353                    | 385        | 462        | 385                         |
| H-H     | 353                    | 385        | 462        | 385                         |
| I-I     | 353                    | 385        | 462        | 385                         |
| J-J     | 353                    | 385        | 462        | 385                         |
| K-K     | 353                    | 385        | 462        | 385                         |
| L-L     | 353                    | 385        | 462        | 385                         |
| M-M     | 353                    | 385        | 462        | 385                         |
| N-N     | 353                    | 385        | 462        | 385                         |
| O-O     | 353                    | 385        | 462        | 385                         |
| P-P     | 353                    | 385        | 462        | 385                         |
| Q-Q     | 353                    | 385        | 462        | 385                         |
| R-R     | 353                    | 385        | 462        | 385                         |
| S-S     | 353                    | 385        | 462        | 385                         |
| T-T     | 353                    | 385        | 462        | 385                         |
| U-U     | 353                    | 385        | 462        | 385                         |
| V-V     | 353                    | 385        | 462        | 385                         |
| W-W     | 353                    | 385        | 462        | 385                         |
| X-X     | 353                    | 385        | 462        | 385                         |
| Y-Y     | 353                    | 385        | 462        | 385                         |
| Z-Z     | 353                    | 385        | 462        | 385                         |
| AA-AA   | 353                    | 385        | 462        | 385                         |
| BB-BB   | 353                    | 385        | 462        | 385                         |
| CC-CC   | 353                    | 385        | 462        | 385                         |
| DD-DD   | 353                    | 385        | 462        | 385                         |
| EE-EE   | 353                    | 385        | 462        | 385                         |
| FF-FF   | 353                    | 385        | 462        | 385                         |
| GG-GG   | 353                    | 385        | 462        | 385                         |
| HH-HH   | 353                    | 385        | 462        | 385                         |
| II-II   | 353                    | 385        | 462        | 385                         |
| JJ-JJ   | 353                    | 385        | 462        | 385                         |
| KK-KK   | 353                    | 385        | 462        | 385                         |
| LL-LL   | 353                    | 385        | 462        | 385                         |
| MM-MM   | 353                    | 385        | 462        | 385                         |
| NN-NN   | 353                    | 385        | 462        | 385                         |
| OO-OO   | 353                    | 385        | 462        | 385                         |
| PP-PP   | 353                    | 385        | 462        | 385                         |
| QQ-QQ   | 353                    | 385        | 462        | 385                         |
| RR-RR   | 353                    | 385        | 462        | 385                         |
| SS-SS   | 353                    | 385        | 462        | 385                         |
| TT-TT   | 353                    | 385        | 462        | 385                         |
| UU-UU   | 353                    | 385        | 462        | 385                         |
| VV-VV   | 353                    | 385        | 462        | 385                         |
| WW-WW   | 353                    | 385        | 462        | 385                         |
| XX-XX   | 353                    | 385        | 462        | 385                         |
| YY-YY   | 353                    | 385        | 462        | 385                         |
| ZZ-ZZ   | 353                    | 385        | 462        | 385                         |
| AAA-AAA | 353                    | 385        | 462        | 385                         |
| BBB-BBB | 353                    | 385        | 462        | 385                         |
| CCC-CCC | 353                    | 385        | 462        | 385                         |
| DDD-DDD | 353                    | 385        | 462        | 385                         |
| EEE-EEE | 353                    | 385        | 462        | 385                         |
| FFF-FFF | 353                    | 385        | 462        | 385                         |
| GGG-GGG | 353                    | 385        | 462        | 385                         |
| HHH-HHH | 353                    | 385        | 462        | 385                         |
| III-III | 353                    | 385        | 462        | 385                         |
| JJJ-JJJ | 353                    | 385        | 462        | 385                         |
| KKK-KKK | 353                    | 385        | 462        | 385                         |
| LLL-LLL | 353                    | 385        | 462        | 385                         |
| MMM-MMM | 353                    | 385        | 462        | 385                         |
| NNN-NNN | 353                    | 385        | 462        | 385                         |
| OOO-OOO | 353                    | 385        | 462        | 385                         |
| PPP-PPP | 353                    | 385        | 462        | 385                         |
| QQQ-QQQ | 353                    | 385        | 462        | 385                         |
| RRR-RRR | 353                    | 385        | 462        | 385                         |
| SSS-SSS | 353                    | 385        | 462        | 385                         |
| TTT-TTT | 353                    | 385        | 462        | 385                         |
| UUU-UUU | 353                    | 385        | 462        | 385                         |
| VVV-VVV | 353                    | 385        | 462        | 385                         |
| WWW-WWW | 353                    | 385        | 462        | 385                         |
| XXX-XXX | 353                    | 385        | 462        | 385                         |
| YYY-YYY | 353                    | 385        | 462        | 385                         |
| ZZZ-ZZZ | 353                    | 385        | 462        | 385                         |
| AAA-AAA | 353                    | 385        | 462        | 385                         |
| BBB-BBB | 353                    | 385        | 462        | 385                         |
| CCC-CCC | 353                    | 385        | 462        | 385                         |
| DDD-DDD | 353                    | 385        | 462        | 385                         |
| EEE-EEE | 353                    | 385        | 462        | 385                         |
| FFF-FFF | 353                    | 385        | 462        | 385                         |
| GGG-GGG | 353                    | 385        | 462        | 385                         |
| HHH-HHH | 353                    | 385        | 462        | 385                         |
| III-III | 353                    | 385        | 462        | 385                         |
| JJJ-JJJ | 353                    | 385        | 462        | 385                         |
| KKK-KKK | 353                    | 385        | 462        | 385                         |
| LLL-LLL | 353                    | 385        | 462        | 385                         |
| MMM-MMM | 353                    | 385        | 462        | 385                         |
| NNN-NNN | 353                    | 385        | 462        | 385                         |
| OOO-OOO | 353                    | 385        | 462        | 385                         |
| PPP-PPP | 353                    | 385        | 462        | 385                         |
| QQQ-QQQ | 353                    | 385        | 462        | 385                         |
| RRR-RRR | 353                    | 385        | 462        | 385                         |
| SSS-SSS | 353                    | 385        | 462        | 385                         |
| TTT-TTT | 353                    | 385        | 462        | 385                         |
| UUU-UUU | 353                    | 385        | 462        | 385                         |
| VVV-VVV | 353                    | 385        | 462        | 385                         |
| WWW-WWW | 353                    | 385        | 462        | 385                         |
| XXX-XXX | 353                    | 385        | 462        | 385                         |
| YYY-YYY | 353                    | 385        | 462        | 385                         |
| ZZZ-ZZZ | 353                    | 385        | 462        | 385                         |



LONGITUDINAL SECTION ON C BEAM



Bottom of beam elevations are given of points marked \*

ELEVATIONS

Bridge over Lost Creek Sheet 14  
**COMMONWEALTH OF KENTUCKY**  
 DEPARTMENT OF HIGHWAYS  
 FRANKFORT  
 COUNTY OF UNION  
 UNIONTOWN-SPRING GROVE  
 ROAD  
 STATION 5+73  
 BRIDGE NUMBER 12520 PROJECT NO. S345(2)